

I:\SEP-2020_09\26
 S:\Shared\Division One Resurfacing & Retreatment Plans\2020-2021\Resurfacing\DA00478-Washington US 64 & Secondary-Hyde & Tyrrell\Secondary\Design Files\DA00478_Dlddc.tspi-4.dwg
 \$\$\$USERNAME\$\$\$

CONTRACT: DA00478 WBS ELEMENT: 2021CPT.01.03.10941.1, ETC.

STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

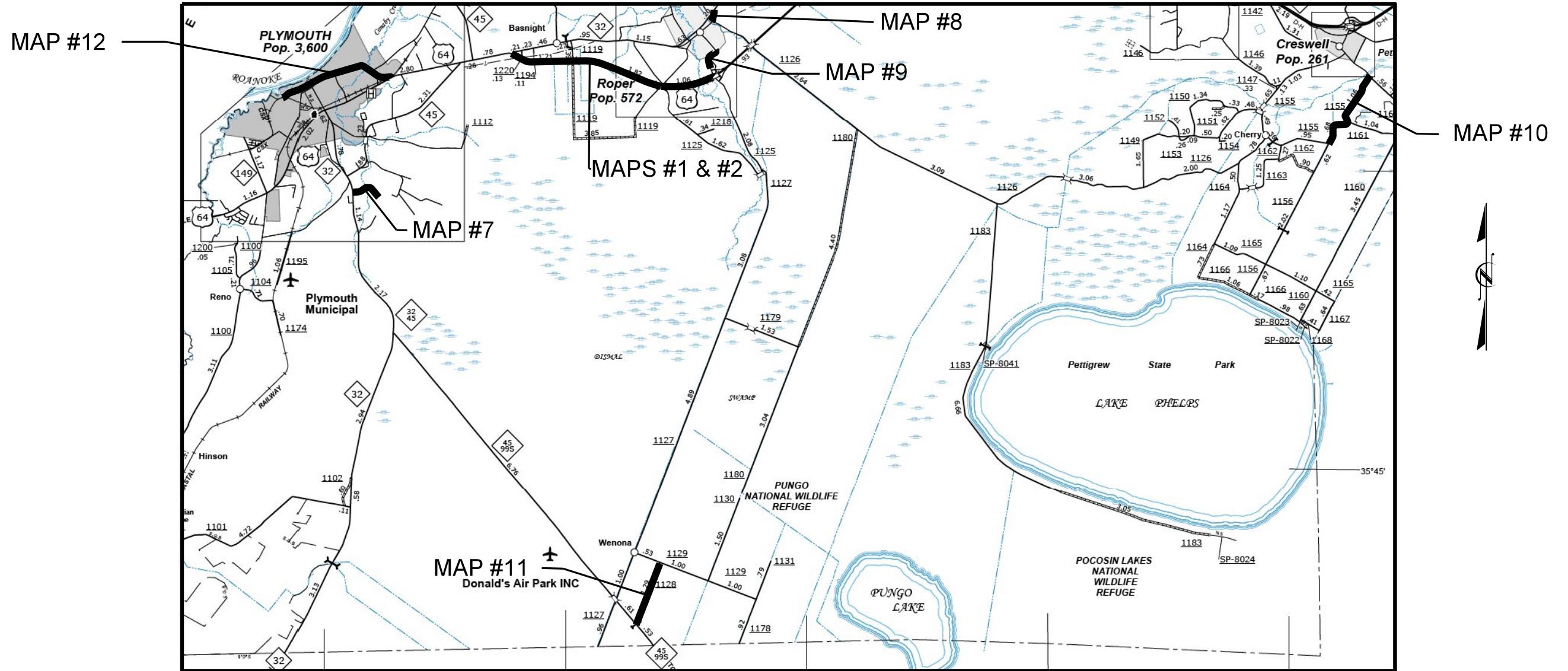
STATE PROJECT REFERENCE NO.	SHEET NO.
N.C. 2021CPT.01.03.10941.1, ETC.	1
STATE PROJ. NO.	DESCRIPTION
2021CPT.01.03.10941.1	PE, CONST
2021CPT.01.03.20941.1	PE, CONST

WASHINGTON COUNTY

LOCATION:

<p><u>MAP #1 & #2 - US 64 EBL & WBL FROM END 5 LANE TO SR 1125 OVERPASS</u></p> <p><u>MAP #7 - SR 1117 (STYONS RD.) FROM DEAD END TO NC 32</u></p> <p><u>MAP #8 - SR 1122 (OLD US 64) FROM BRIDGE OVER MAIN CANAL TO NC 32</u></p> <p><u>MAP #9 - SR 1125 (MILL POND RD.) FROM PAV. CHANGE AT M.P. 0.3 TO SR 1175</u></p>	<p><u>MAP #10 - SR 1155 (OLD CHERRY RD.) FROM SR 1142 TO SR 1156</u></p> <p><u>MAP #11 - SR 1128 (A CANAL RD.) FROM SR 1129 TO NC 45</u></p> <p><u>MAP #12 - SR 1325 (MAIN ST.) FROM US 64 TO DEAD END</u></p>
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TYPE OF WORK: AST (DOUBLE SEAL), OGAF, MILLING & RESURFACING



GRAPHIC SCALES

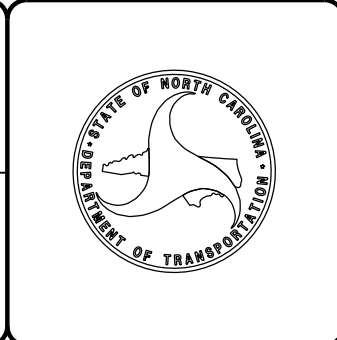
NTS

PROJECT LENGTH	
MAP #1 =	3.93 MILES
MAP #2 =	3.93 MILES
MAP #7 =	0.61 MILES
MAP #8 =	0.21 MILES
MAP #9 =	0.34 MILES
MAP #10 =	1.73 MILES
MAP #11 =	1.28 MILES
MAP #12 =	2.32 MILES

Prepared in the Office of:
DIVISION OF HIGHWAYS
113 Airport Dr., Edenton NC, 27944

2018 STANDARD SPECIFICATIONS	<p style="text-align: center;">W. B. HOBBS, PE DIVISION PROJECT TEAM LEAD</p> <hr/> <p style="text-align: center;">CHRIS SLACHTA DIVISION PROPOSALS ENGINEER</p>
------------------------------	--

S. P. FENWICK, PE
DIVISION DESIGN ENGINEER



I:\SEP-2020_09\31 S:\Shared\Division One Resurfacing & Retreatment Plans\2020-2021\Resurfacing\DA00478-Washington US 64 & Secondary_Hyde & Tyrrell\Secondary\Design Files\DA00478_Dliddc.tspi-4.dwg
 \$\$\$USERNAME\$\$\$
 09/08/20

CONTRACT: DA00478 WBS PROJECT: 2021CPT.01.03.10941.1, ETC.

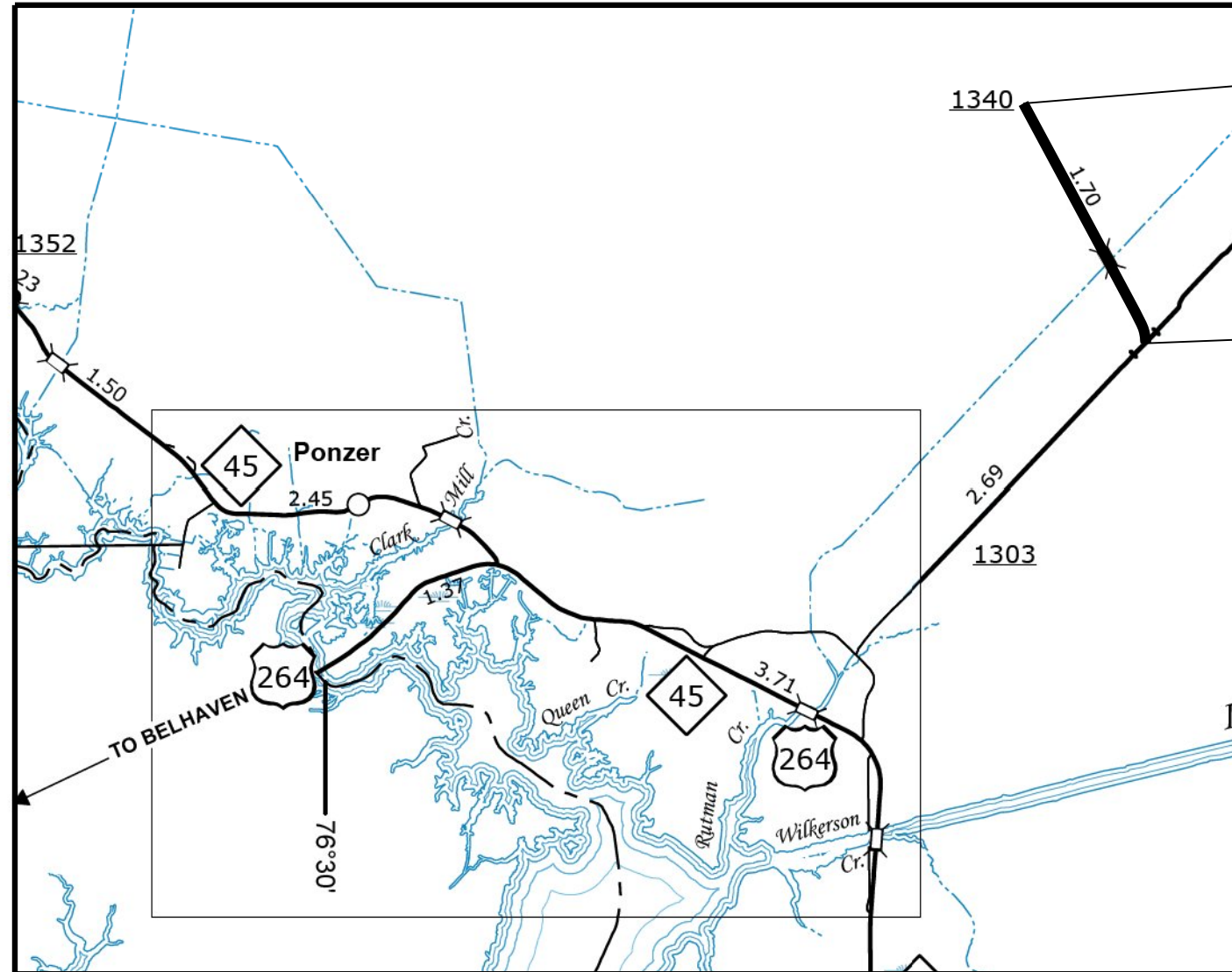
STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS

HYDE COUNTY

LOCATION: SR 1340 (GALL RD) FROM SR 1303 (NEW LAKE RD) TO DEAD END

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.
N.C.	2021CPT.01.03.10941.1, ETC.	2
STATE PROJ. NO.		DESCRIPTION
2021CPT.01.03.20481.1		PE, CONST.

TYPE OF WORK: RESURFACING



END MAP 3
 AT DEAD END

BEGIN MAP 3
 AT SR 1303 (NEW LAKE RD)

GRAPHIC SCALES

NTS

PROJECT LENGTH

LENGTH ROADWAY MAP #3 = 1.56 MILES

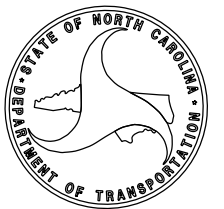
Prepared in the Office of:
DIVISION OF HIGHWAYS
 113 AIRPORT DR., EDENTON, NC, 27932

2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E.
 DIVISION PROJECT TEAM LEAD

CHRIS SLACHTA
 DIVISION PROPOSALS ENGINEER

S.P. FENWICK, PLS
 DESIGN ENGINEER



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CONTRACT: DA00478 WBS PROJECT: 2021CPT.01.03.10941.1, ETC.

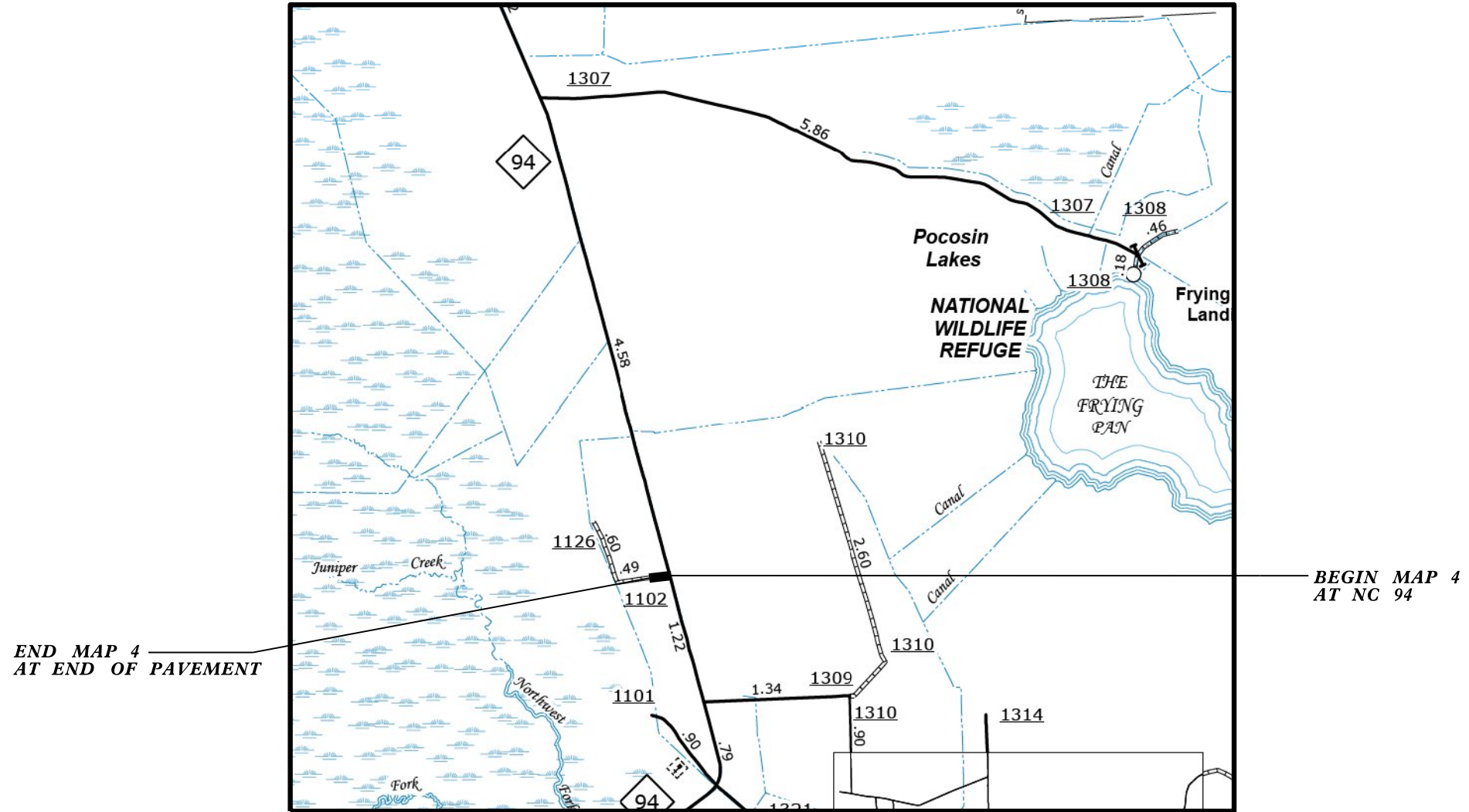
STATE OF NORTH CAROLINA
 DIVISION OF HIGHWAYS

TYRRELL COUNTY

LOCATION: SR 1102 (JONES RD) FROM NC 94 TO END OF PAVEMENT

TYPE OF WORK: RESURFACING

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.
N.C.	2021CPT.01.03.10941.1, ETC.	3
STATE PROJ. NO.		DESCRIPTION
2021CPT.01.03.20891.1		PE, CONST



GRAPHIC SCALES

NTS

PROJECT LENGTH

LENGTH ROADWAY MAP #4 = 0.1 MILES

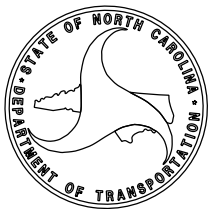
Prepared in the Office of:
DIVISION OF HIGHWAYS
 113 AIRPORT DR., EDENTON, NC, 27932

2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E.
 DIVISION PROJECT TEAM LEAD

CHRIS SLACHTA
 DIVISION PROPOSALS ENGINEER

S.P. FENWICK, PLS
 DESIGN ENGINEER



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 09/08/19

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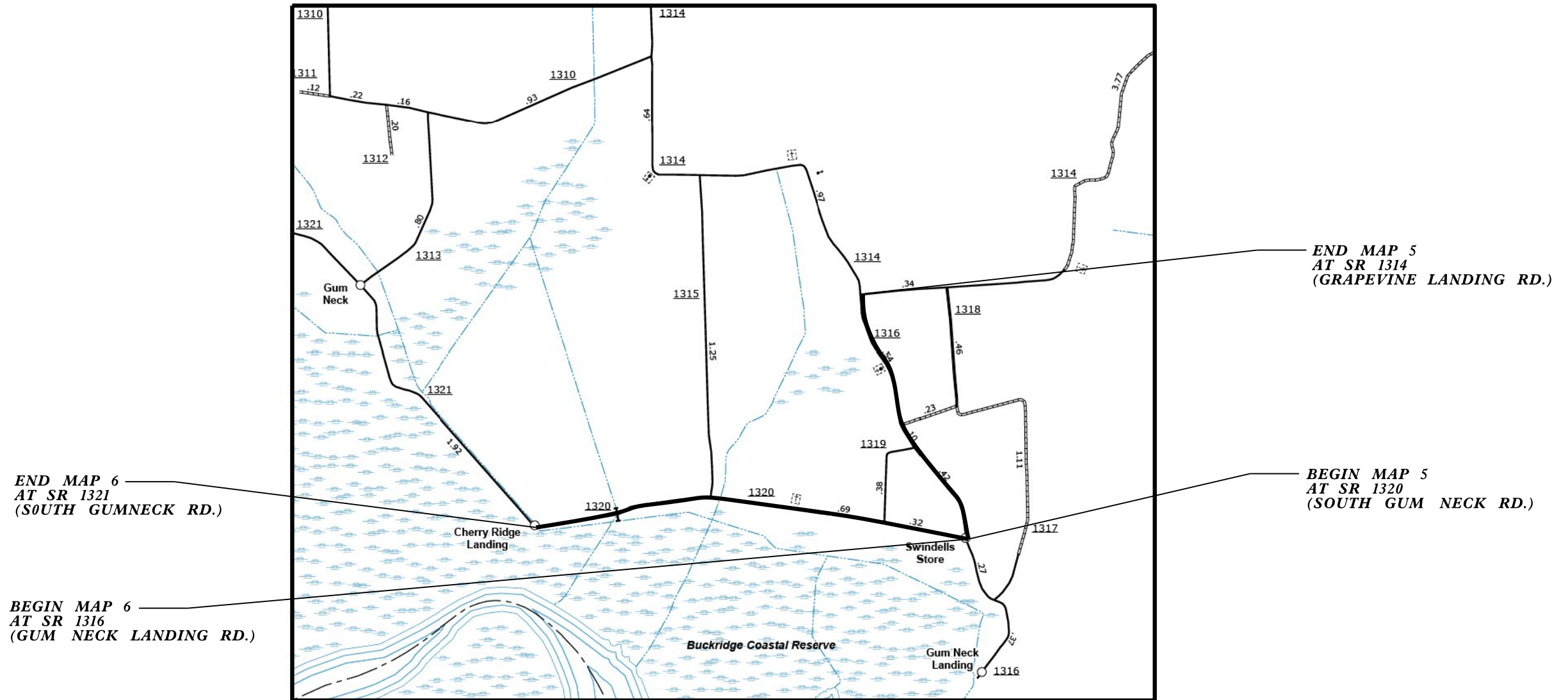
STATE OF NORTH CAROLINA
DIVISION OF HIGHWAYS

STATE	STATE PROJECT REFERENCE NO.	SHEET NO.
N.C.	2021CPT.01.03.10941.1, ETC.	4
	STATE PROJ. NO.	DESCRIPTION
	2021CPT.01.03.20891.1	PE, CONST.

TYRRELL COUNTY

LOCATION: MAP 5 – SR 1316 (GUM NECK LANDING RD) FROM SR 1320 (S GUM NECK RD) TO SR 1314 (GRAPEVINE LANDING RD)
MAP 6 – SR 1320 (S GUM NECK RD) FROM SR 1316 (GUM NECK LANDING RD) TO SR 1321 (S GUM NECK RD)

TYPE OF WORK: RESURFACING



GRAPHIC SCALES

NTS

PROJECT LENGTH

LENGTH ROADWAY MAP #5 = 1.06 MILES
 LENGTH ROADWAY MAP #6 = 1.71 MILES

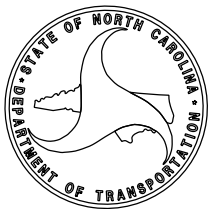
Prepared in the Office of:
DIVISION OF HIGHWAYS
 113 AIRPORT DR., EDENTON, NC, 27932

2018 STANDARD SPECIFICATIONS

W.B. HOBBS, P.E.
 DIVISION PROJECT TEAM LEAD

CHRIS SLACHTA
 DIVISION PROPOSALS ENGINEER

S.P. FENWICK, PLS
 DESIGN ENGINEER



PAVEMENT SCHEDULE

PROJECT REFERENCE NO.	SHEET NO.
2021CPT.01.03,10941.1, ETC.	5

B1	PROP. APPROX. 0.75" OPEN GRADE ASPHALT FRICTION COURSE, TYPE 1 MODIFIED, AT AN AVERAGE RATE OF 90 LBS. PER SQ. YD.
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. VAR. DEPTH ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C, AT AN AVERAGE RATE OF 112 LBS. PER SQ. YD. PER 1" DEPTH. TO BE PLACED IN LAYERS NOT TO EXCEED 2" IN DEPTH.
V1	MILLING ASPHALT PAVEMENT. 1.5" IN DEPTH.
V2	MILLED RUMBLE STRIPS
U	EXISTING PAVEMENT.

NOTES:

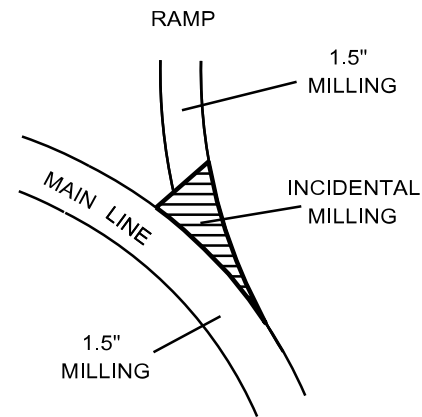
ALL INGRESS AND EGRESS RAMPS TO BE RESURFACED TO THE EDGE OF PAVEMENT OF THE CONNECTING ROAD OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

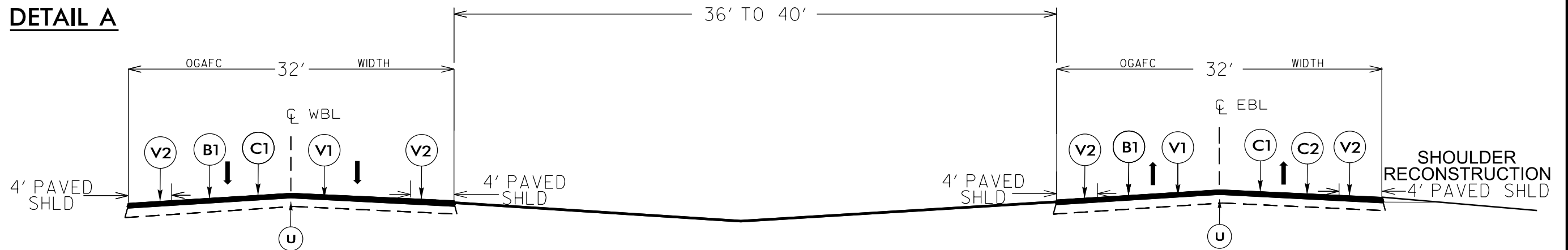
1.5" MILLING AND 1.5" OF S9.5C TO BE APPLIED THE FULL WIDTH OF THE ROADWAY

0.75" OGAF C TO BE APPLIED THE FULL WIDTH OF THE ROADWAY

LEVEL COURSE S9.5C TO BE USED ON US 64 EBL FOR APPROXIMATELY 1000' BEGINNING AT STATION 103+89.00 FROM PAVEMENT JOINT (STATION 0+00.00) AT NC 32. SHOULDER RECONSTRUCTION WILL BE NEEDED FOR THIS AREA.



DETAIL A



TYPICAL SECTION NO. 1
USE WITH MAPS 1 & 2

I-SEP-2020 10:44 S:\Sherrod\Division One Resurfacing & Retreatment Plans\2020-2021 Resurfacing\DA00478-Washington US 64 & Secondary-Hyde & Tyrrell Secondary Design Files\DA00478_D\ddc.sh5-7_9-14.dgn

PAVEMENT SCHEDULE

NOTES:

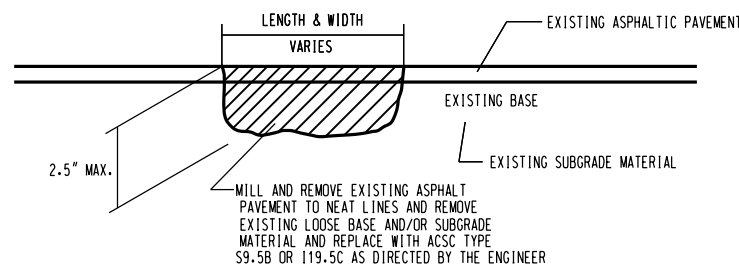
C	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
F	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
U	EXISTING PAVEMENT.
T	EARTH MATERIAL.

ALL PAVED S.R. ROADS OR RAMPS TO BE RESURFACED TO THE ENDS OF THE RADII OR AS DIRECTED BY THE ENGINEER

EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

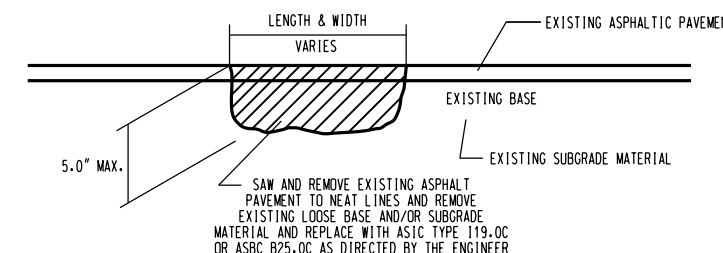
ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE, PRIOR TO FINAL SURFACE LAYER BEING INSTALLED

*ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT: BOTTOM LAYER CONSISTS OF EMULSIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY. TOP LAYER CONSISTS OF EMULSIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.25 + GAL/SY AND A LAYER OF 5/16" LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX. 9+ LBS/SY.



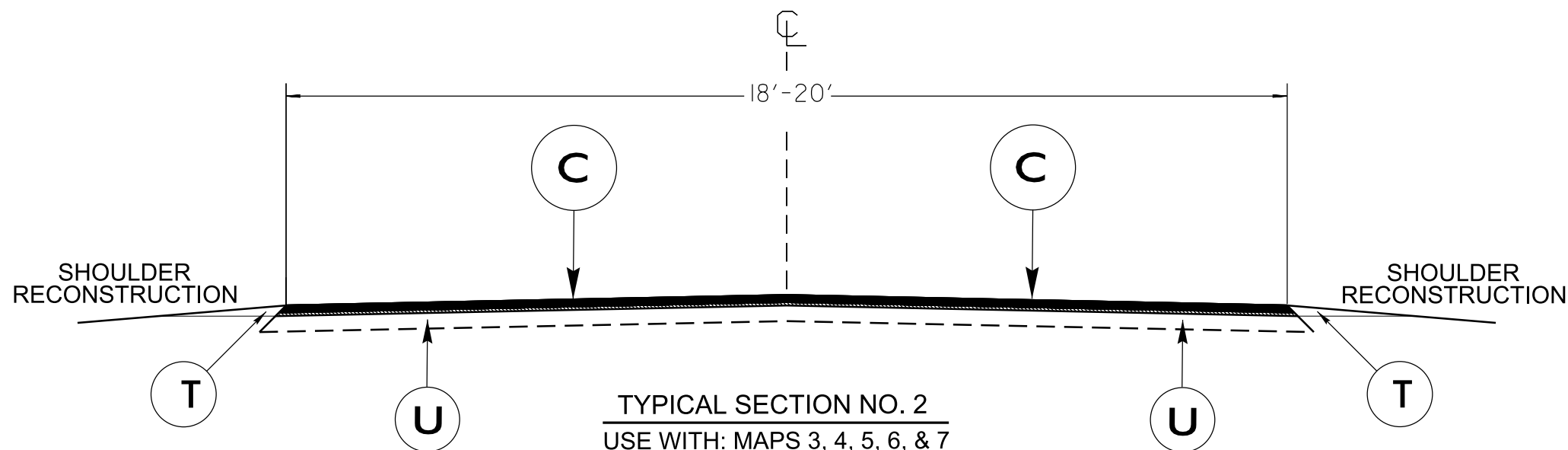
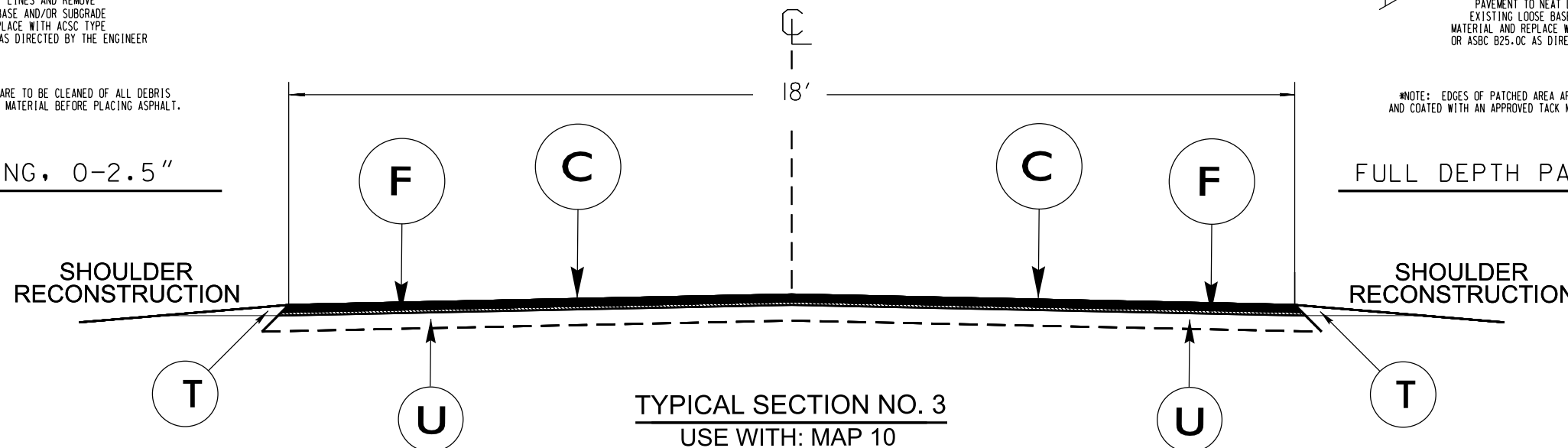
*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

MILL PATCHING, 0-2.5"



*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING 0-5"



PAVEMENT SCHEDULE

NOTES:

C	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B, AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
E	PROP. APPROX. 5.5" ASPHALT CONCRETE BASE COURSE, TYPE B25.0C, AT AN AVERAGE RATE OF 627 LBS. PER SQ. YD.
F	ASPHALT SURFACE TREATMENT, DOUBLE SEAL
U	EXISTING PAVEMENT.
V1	MILLING ASPHALT PAVEMENT. 1.5" IN DEPTH.
T	EARTH MATERIAL.

ALL PAVED S.R. ROADS OR RAMPS TO BE RESURFACED TO THE ENDS OF THE RADII OR AS DIRECTED BY THE ENGINEER

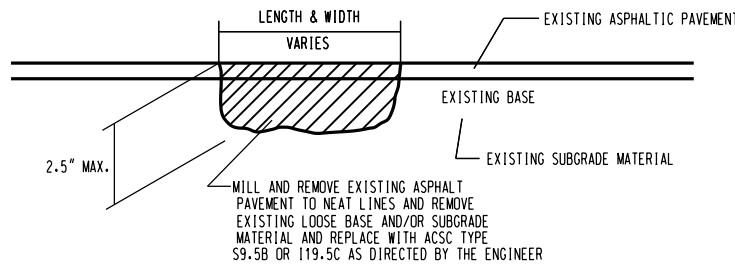
EDGES, PAVEMENT WIDENING, INTERSECTIONS AND BRIDGE FLARES ARE INCLUDED IN THE SUMMARY OF QUANTITIES

ASPHALT SURFACE TREATMENT SHALL BE ALLOWED TO SET UP FOR A MINIMUM OF 10 DAYS BEFORE ANY OTHER WORK CAN BE PERFORMED. ASPHALT SURFACE TREATMENT SHALL BE THOROUGHLY BROOMED TO REMOVE EXCESS AGGREGATE, PRIOR TO FINAL SURFACE LAYER BEING INSTALLED

2' WIDENING IS ON THE WEST SIDE OF ROAD ONLY FOR MAP #11

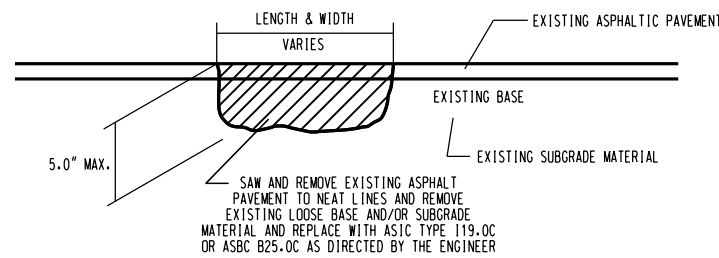
SOME AREAS OF MAP #12 HAVE PAVEMENT IN THE GUTTER AND SHOULD BE PAVED BACK AS THEY EXIST FOR MAPS #8 & #9 WITH CURB AND GUTTER, MILL 1.5" BELOW THE EDGE OF GUTTER PAN.

ASPHALT SURFACE TREATMENT (DOUBLE SEAL) CONSISTS OF TWO LAYERS OF TREATMENT: BOTTOM LAYER CONSISTS OF EMULSIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.30 + GAL/SY AND A LAYER OF 78M AGGREGATE AT A RATE OF APPROX. 18 + LBS/SY. TOP LAYER CONSISTS OF EMULSIFIED ASPHALT, GRADE CRS-2L AT A RATE OF APPROX. 0.25 + GAL/SY AND A LAYER OF 5/16" LIGHTWEIGHT AGGREGATE AT A RATE OF APPROX. 9+ LBS/SY.



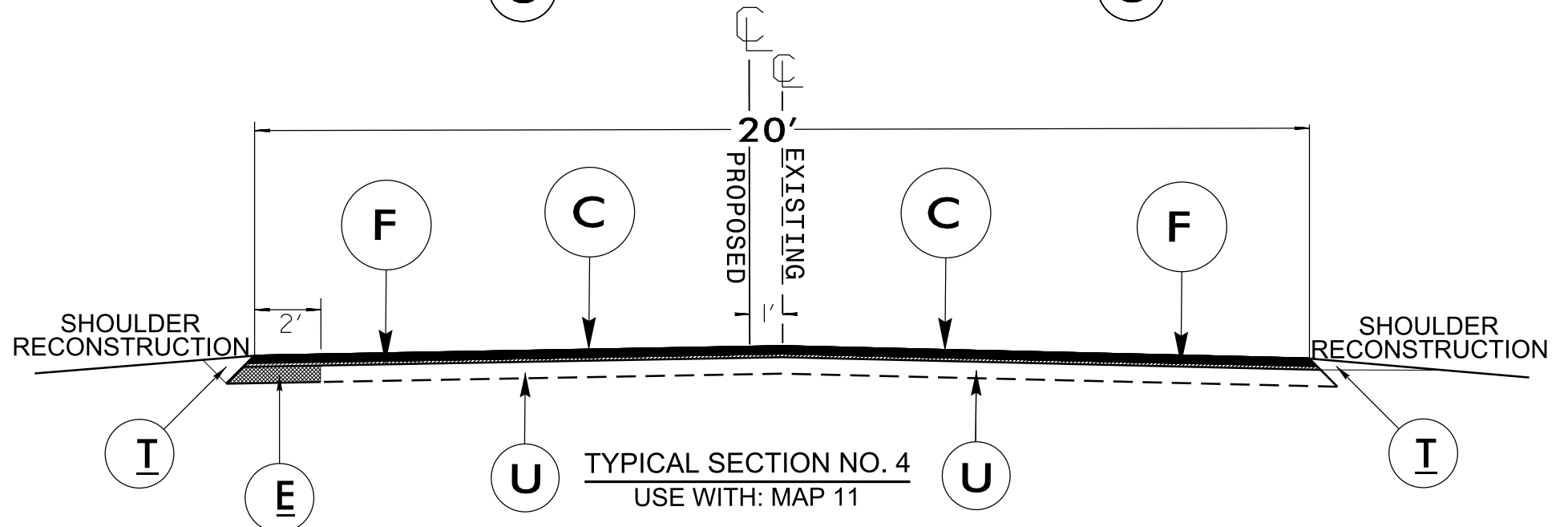
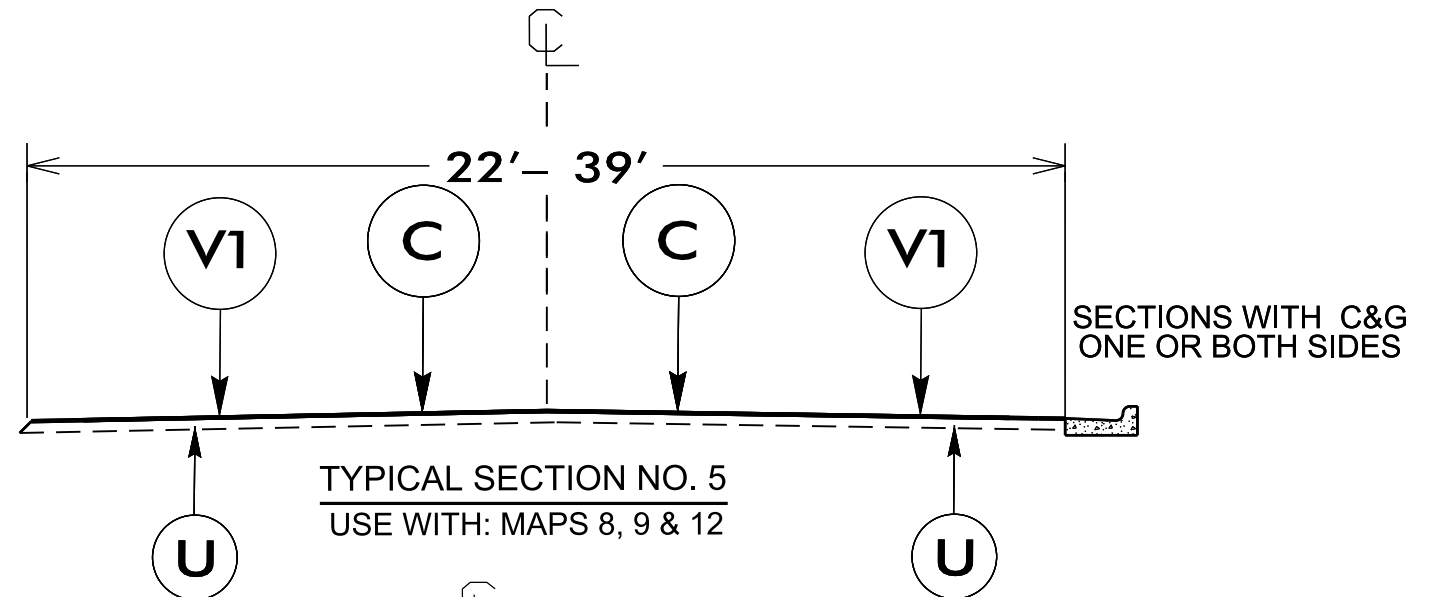
*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

MILL PATCHING, 0-2.5"



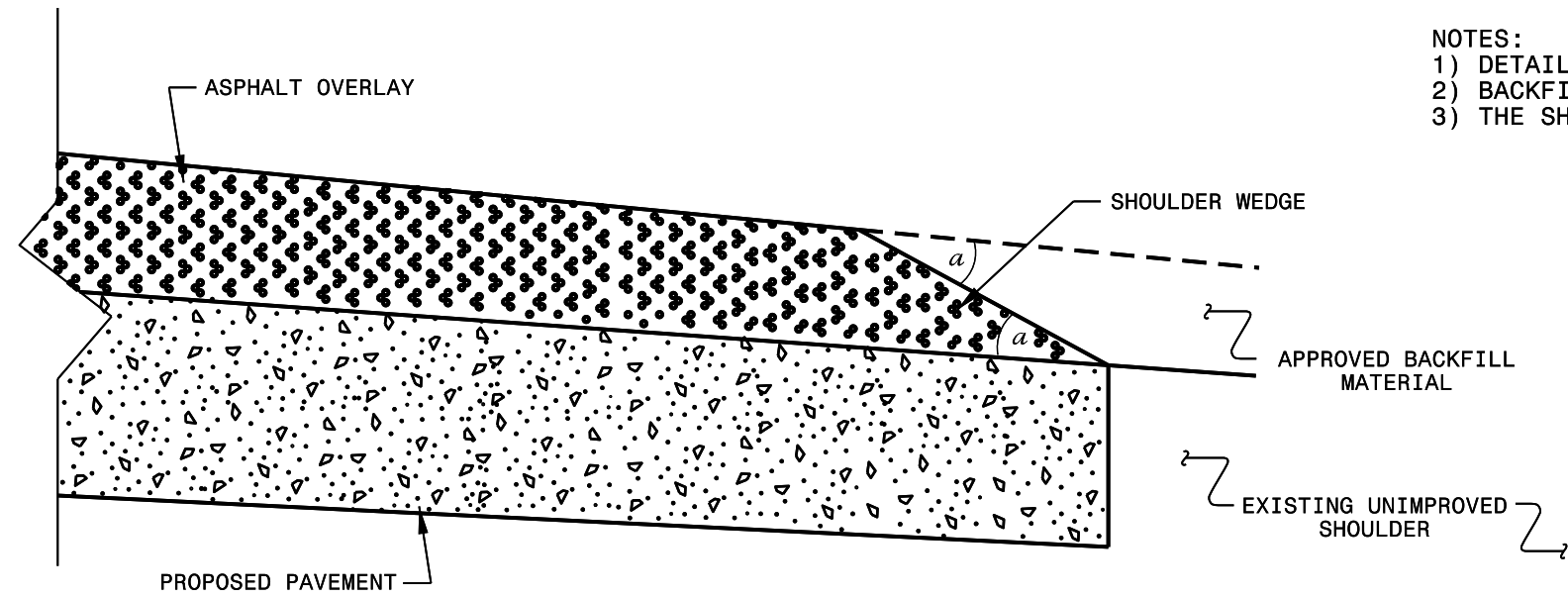
*NOTE: EDGES OF PATCHED AREA ARE TO BE CLEANED OF ALL DEBRIS AND COATED WITH AN APPROVED TACK MATERIAL BEFORE PLACING ASPHALT.

FULL DEPTH PATCHING 0-5"

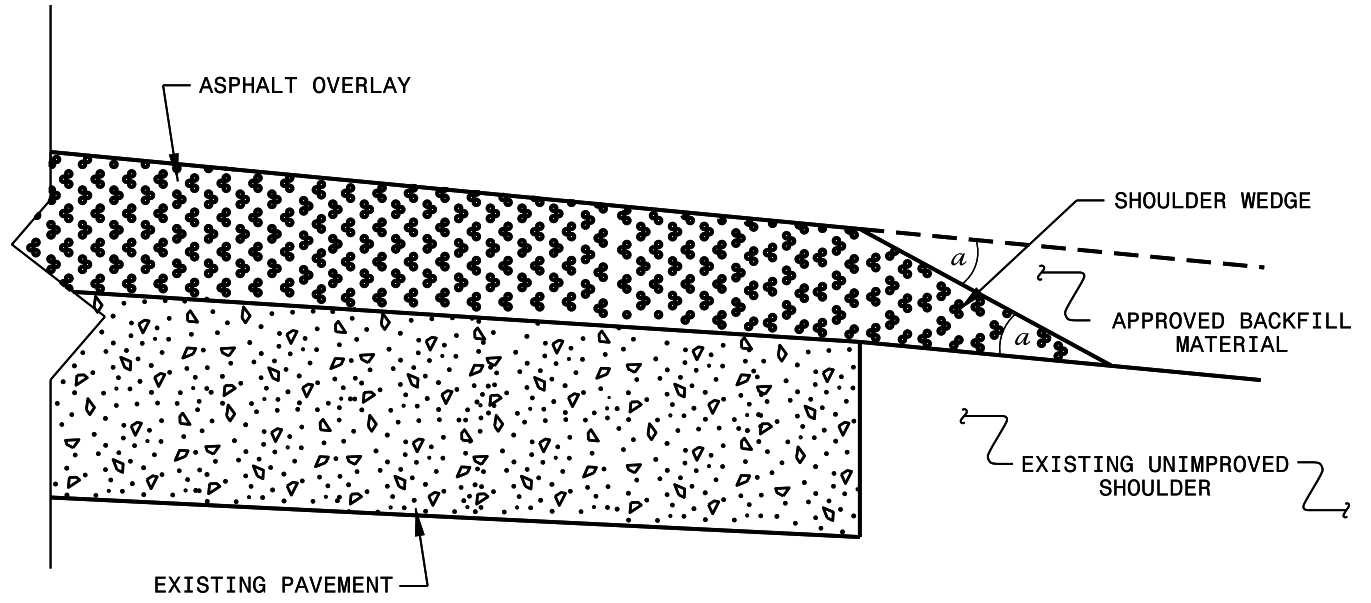


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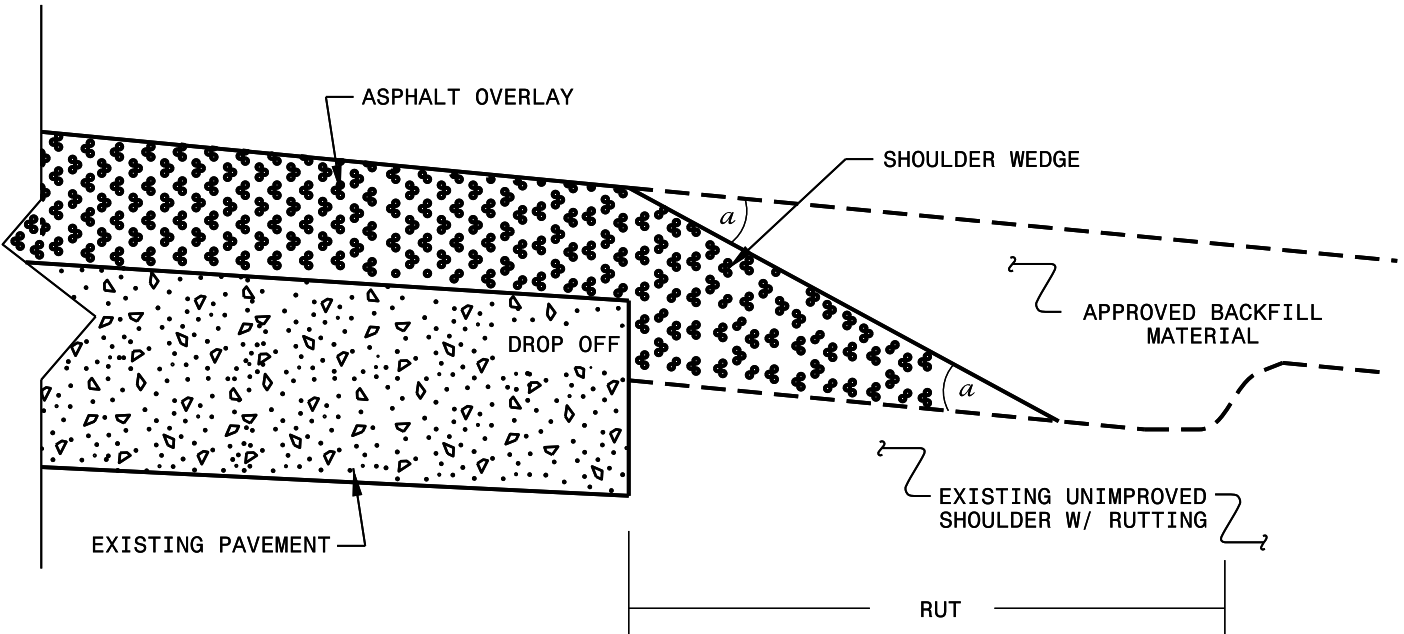
- NOTES:
 1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
 2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
 3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ Widening or
 with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL
 (Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL
 (Resurfacing Adjacent to
 Rutted Shoulder)

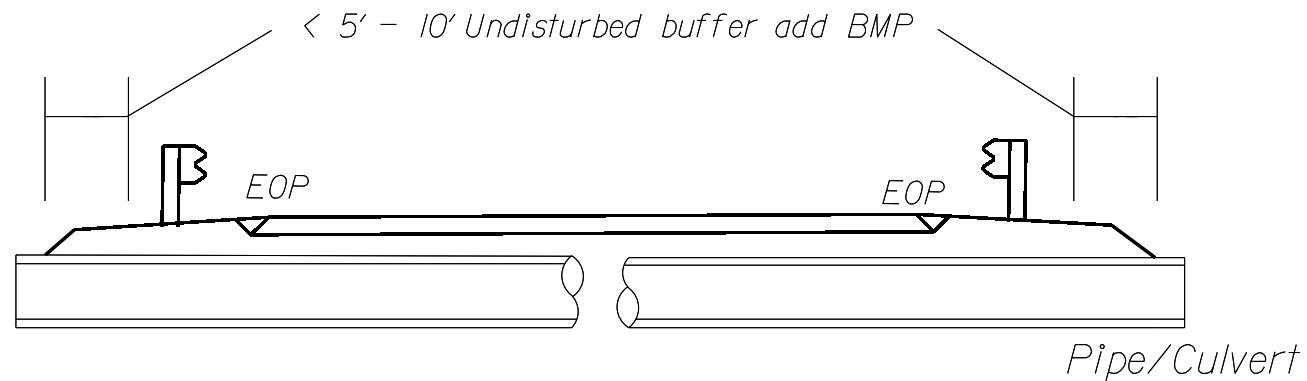
- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT	
Office 919-707-6950	FAX 919-250-4119
SHOULDER WEDGE DETAILS	
ORIGINAL BY: T. SPELL	DATE: 7-19-11
MODIFIED BY:	DATE: 10/16/12
CHECKED BY:	DATE:
FILE SPEC.: s:\usr\details\stand\shoulderwedgedetail.dgn	

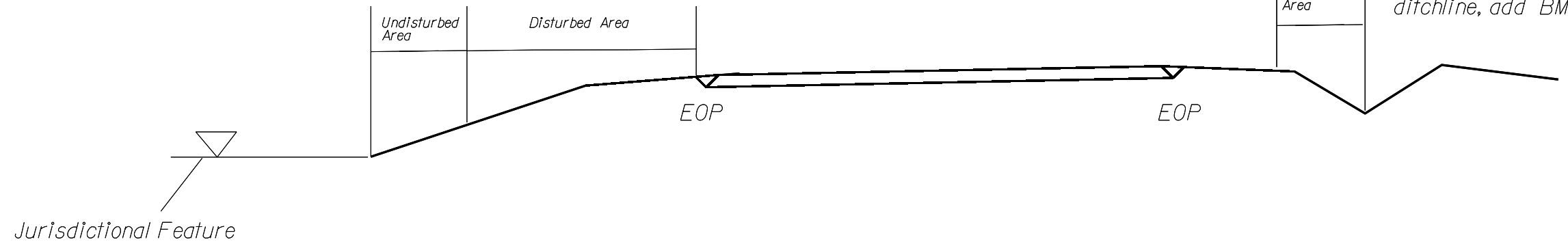
NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle or Silt Fence

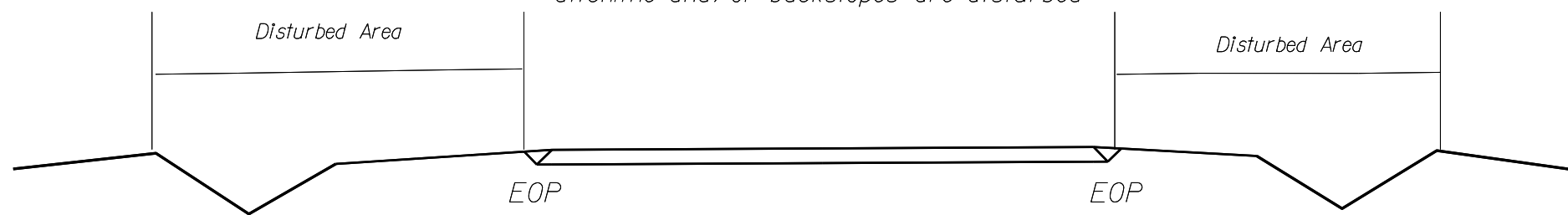
EROSION CONTROL DETAIL



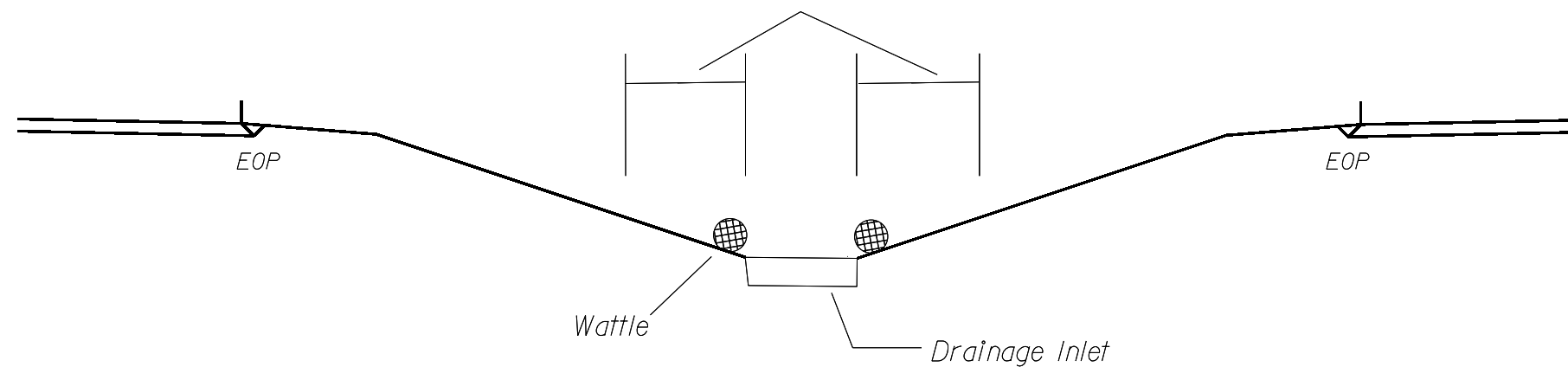
< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed

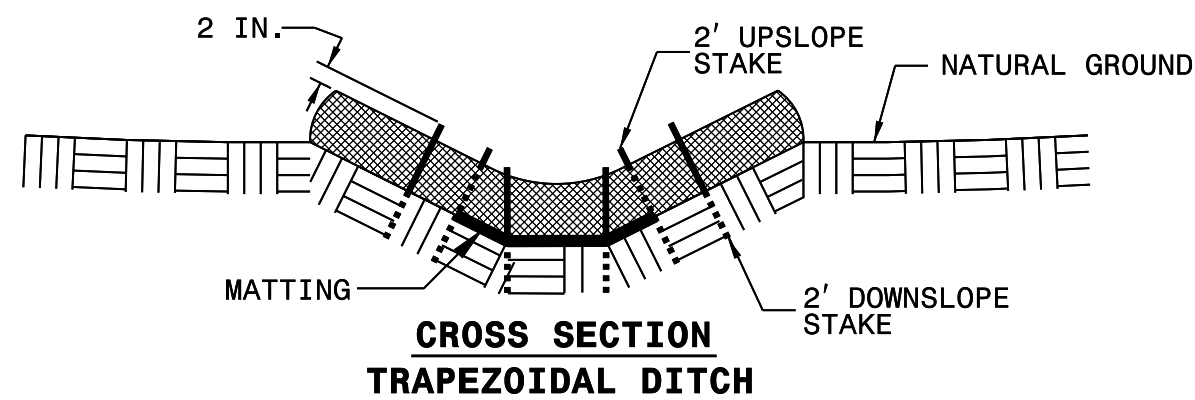
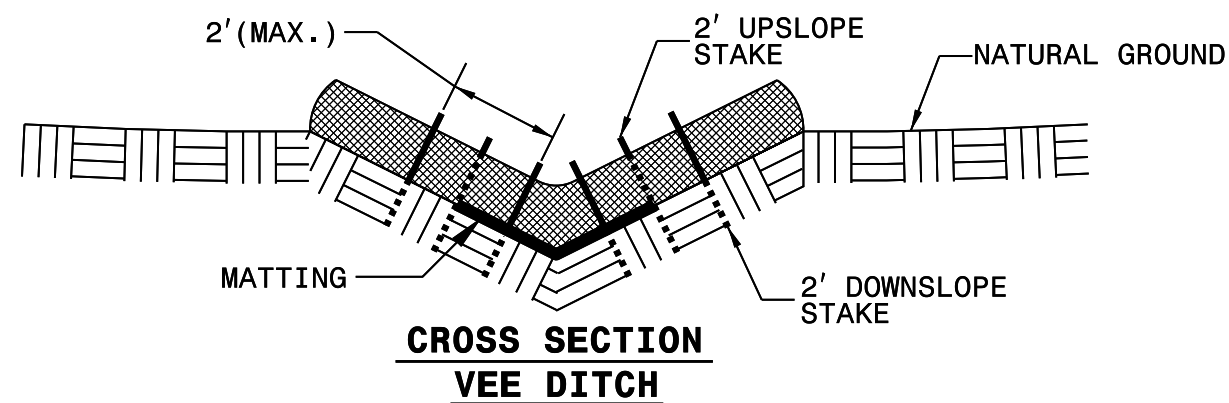
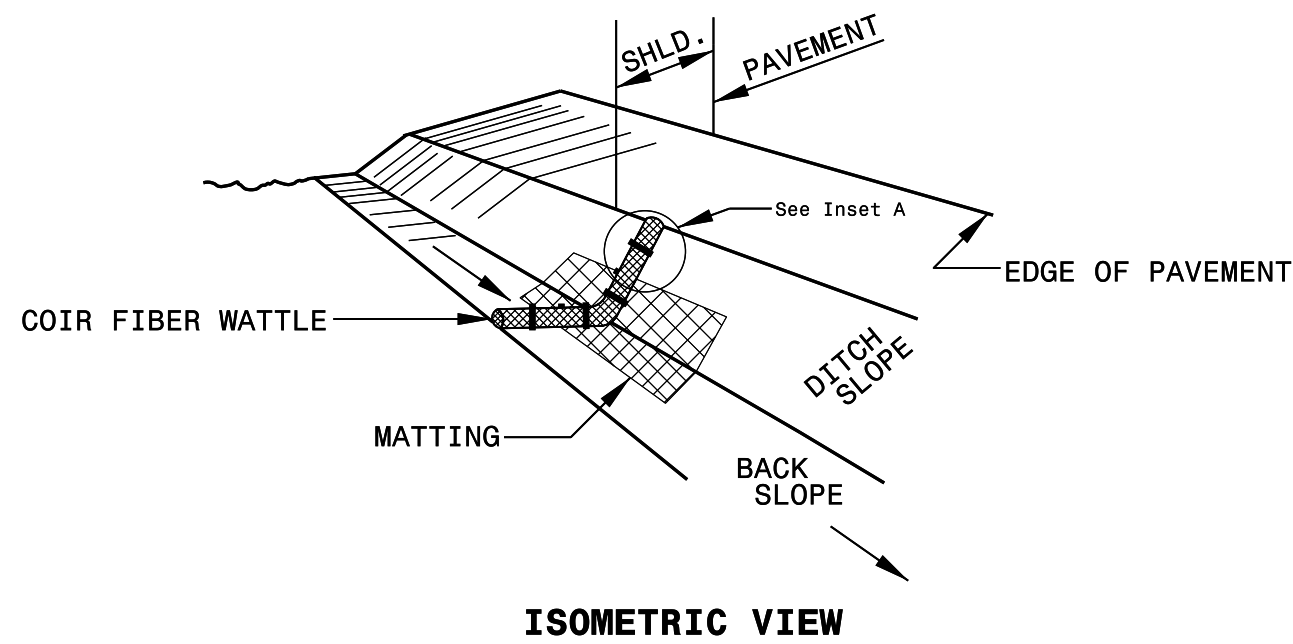


< 5' - 10' Undisturbed buffer from inlet, add wattle

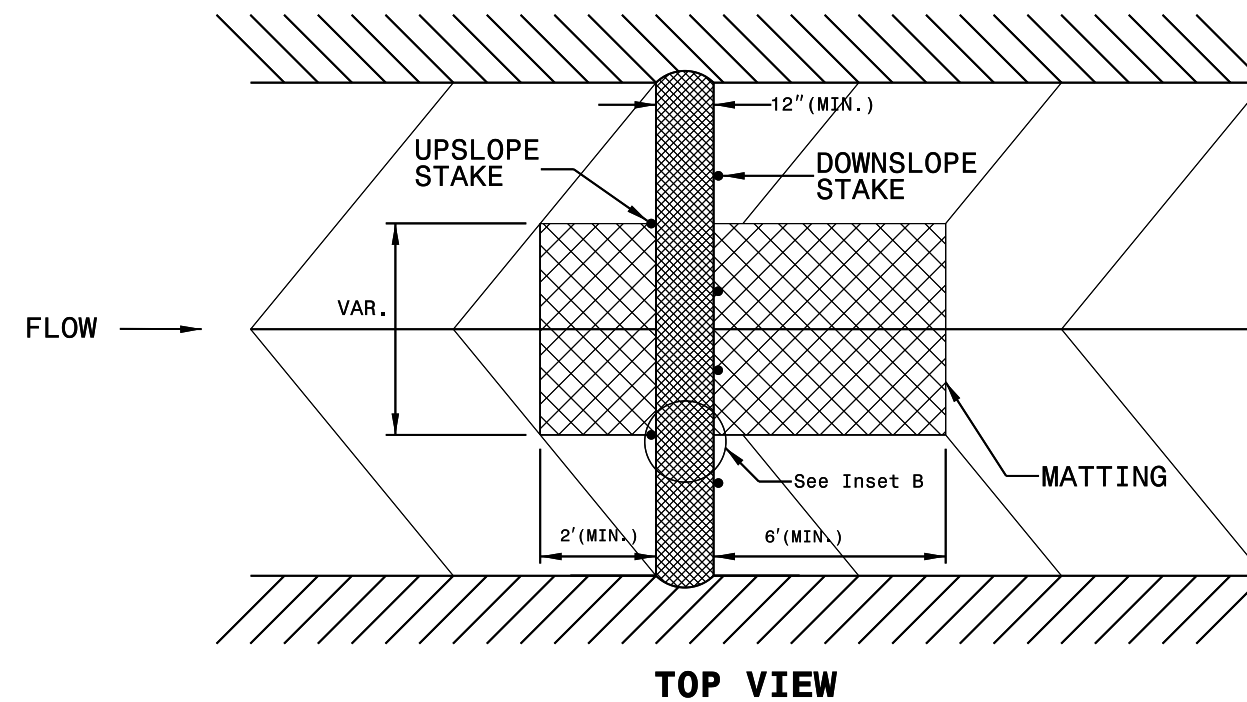
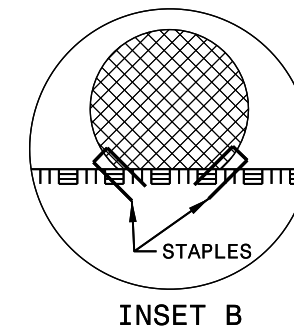
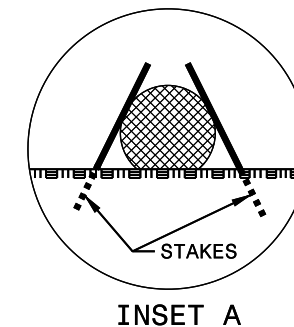


NOT TO SCALE

COIR FIBER WATTLE DETAIL



- NOTES:
- USE MINIMUM 12 IN. DIAMETER COIR FIBER (COCONUT FIBER) WATTLE.
 - USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.
 - ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.
 - INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.
 - PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.
 - INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.
 - INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.




DIVISION OF HIGHWAYS
STATE OF NORTH CAROLINA


SOIL STABILIZATION TIMEFRAMES


<i>SITE DESCRIPTION</i>	<i>STABILIZATION TIME</i>	<i>TIMEFRAME EXCEPTIONS</i>
PERIMETER DIKES, SWALES, DITCHES AND SLOPES	7 DAYS	NONE
HIGH QUALITY WATER (HQW) ZONES	7 DAYS	NONE
SLOPES STEEPER THAN 3:1	7 DAYS	IF SLOPES ARE 10' OR LESS IN LENGTH AND ARE NOT STEEPER THAN 2:1, 14 DAYS ARE ALLOWED.
SLOPES 3:1 OR FLATTER	14 DAYS	7 DAYS FOR SLOPES GREATER THAN 50' IN LENGTH.
ALL OTHER AREAS WITH SLOPES FLATTER THAN 4:1	14 DAYS	NONE, EXCEPT FOR PERIMETERS AND HQW ZONES.

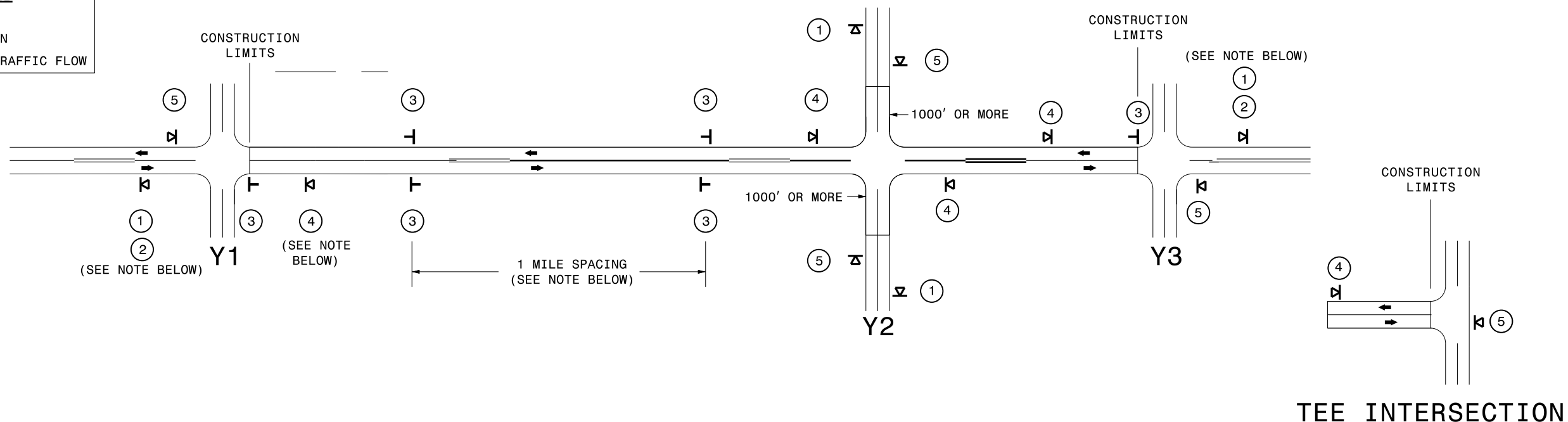
SIGNING FOR ASPHALT SURFACE TREATMENT

LEGEND

 PORTABLE SIGN








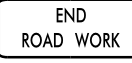
 STATIONARY SIGN

 DIRECTION OF TRAFFIC FLOW



MAINLINE (-L-) SIGNING

-Y- LINE SIGNING



SIGNING NOTES AND PLACEMENT PER DIRECTION	①	 <small>W20-1 48" X 48"</small>	<ul style="list-style-type: none"> - PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE. 	<p>STATIONARY SIGNING NOT REQUIRED FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around; align-items: center;"> <div style="text-align: center;">  <small>W20-1 48" X 48"</small> PLACED 500' IN ADVANCE OF FLAGGER. </div> <div style="text-align: center;">  <small>W20-7 A 48" X 48"</small> PLACED 250' IN ADVANCE OF FLAGGER. </div> </div>
	②	 <small>W7-3aP 24" X 18"</small>	<ul style="list-style-type: none"> - SIGN #2 ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO THE NEAREST WHOLE NUMBER. DO NOT USE FRACTIONAL OR DECIMAL NUMBERS. 	
	③	 <small>W8-7 48" X 48"</small>  <small>SP 48" X 48"</small>	<ul style="list-style-type: none"> - ALTERNATE THE FOLLOWING TWO SIGNS: - STARTING WITH "LOOSE GRAVEL" (W8-7) FOLLOWED BY "UNMARKED PAVEMENT". - PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY 0.5 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER. 	
	④	 <small>SP 13106 48" X 48"</small>	<ul style="list-style-type: none"> - THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. 	
	⑤	 <small>G20-2 A 48" X 24"</small>	<ul style="list-style-type: none"> - PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION. 	
<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>				
<p>FOR AST RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, USE A STATIONARY "LOOSE GRAVEL" SIGN AT THE BEGINNING CONSTRUCTION LIMIT FOLLOWED BY AN "UNMARKED PAVEMENT" SIGN MIDWAY THROUGH AND AN "END ROAD WORK" SIGN AT THE END CONSTRUCTION LIMIT.</p>				

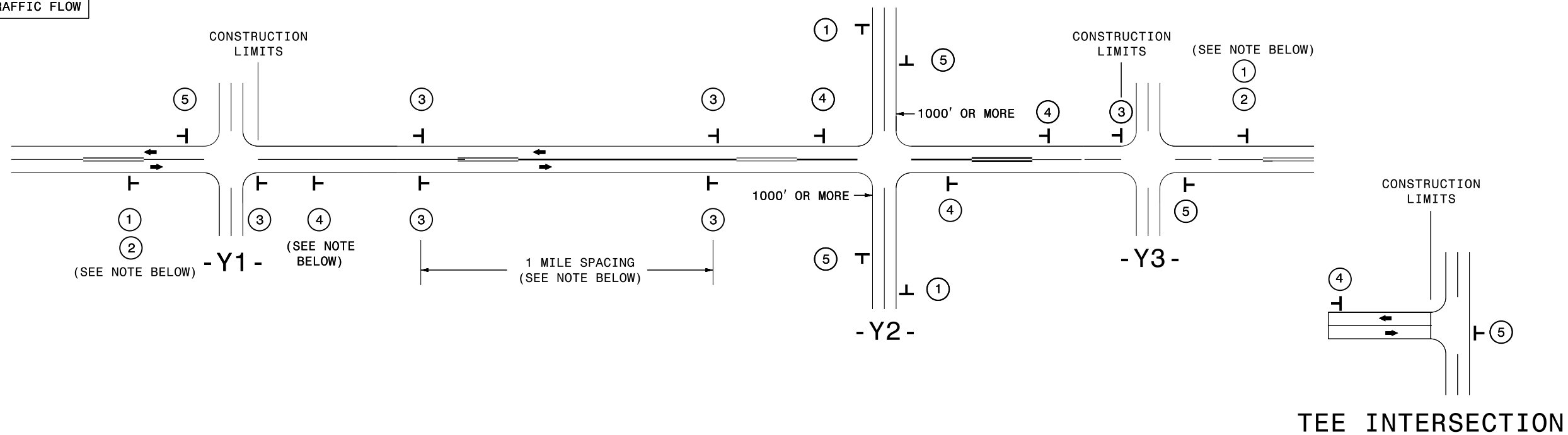
MAPS LESS THAN 2 MILES



ADVANCE WARNING SIGNS FOR 2-LANE ROADWAY ASPHALT SURFACE TREATMENT








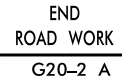
SIGNING FOR RESURFACING PROJECTS

LEGEND
 STATIONARY SIGN
 DIRECTION OF TRAFFIC FLOW



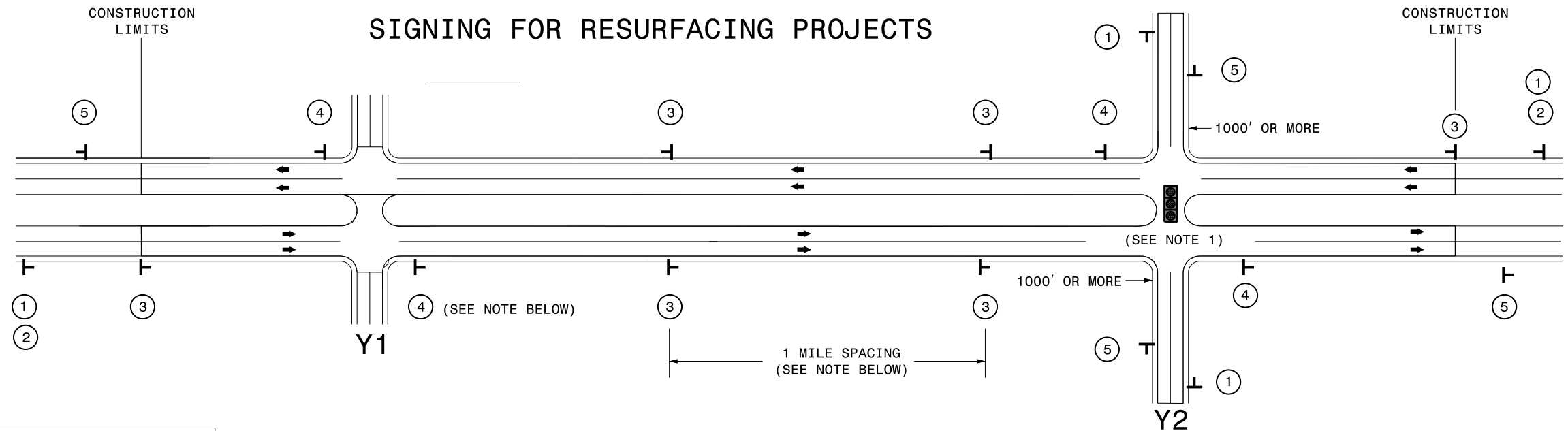
MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	<p>①  W20-1 48" X 48"</p> <p>②  W7-3aP 24" X 18"</p>	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN CONSTRUCTION LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS <p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, PORTABLE ADVANCE WARNING SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p> <div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">  W20-1 48" X 48" </div> <div style="text-align: center;">  W20-7 A 48" X 48" </div> </div> <p>PLACED 500' IN ADVANCE OF FLAGGER.</p>
	<p>③  SP 13107 48" X 48"</p> <p> SP 48" X 48"</p>	<p>- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER.</p> <p>- AT TEE INTERSECTIONS INSTALL INITIALLY 1/2 MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.</p>	
	<p>④  SP 13106 48" X 48"</p>	<p>- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS.</p> <p>- DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS.</p> <p>- INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE.</p> <p>- FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH.</p> <p>- A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p> <p>- FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.</p>	
	<p>⑤  G20-2 A 48" X 24"</p>	<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.</p>	
	<p>THE ABOVE SIGNS ARE ALL THAT ARE REQUIRED FOR A CONTRACTOR TO BEGIN A RESURFACING CONTRACT. ANY ADDITIONAL SIGNS REQUESTED BY NCDOT DIVISIONS SHALL BE INSTALLED WITHIN 7 BUSINESS DAYS OF THE START OF CONTRACT WORK.</p>		
<p>MAPS LESS THAN 2 MILES</p>	<p>FOR RESURFACING MAPS WITH CONSTRUCTION LIMITS LESS THAN 2 MILES IN LENGTH, NO STATIONARY SIGNS ARE REQUIRED. USE PORTABLE "ROAD UNDER CONSTRUCTION" OR "ROAD WORK AHEAD" SIGNS IN LIEU OF STATIONARY ADVANCE WARNING SIGNS.</p>		



ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2-LANE ROADWAY RESURFACING



LEGEND	
T	STATIONARY SIGN
←	DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

-Y- LINE SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION	MAINLINE (-L-) SIGNING		-Y- LINE SIGNING	
	① ②	 	<p>PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE.</p> <p>#2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER. (NO FRACTIONAL OR DECIMAL NUMBERS)</p>	<p>NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:</p> <ol style="list-style-type: none"> 1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE 2) SUBDIVISION ROADS 3) DEAD END ROADS
	③		<p>PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACED 1 MILE APART THEREAFTER. IF NO -Y- LINES EXIST, PLACE 2ND SET 1/2 MILE FROM THE CONSTRUCTION LIMITS AND THEN SPACE 1 MILE THEREAFTER.</p>	<p>WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.</p>
	④		<p>THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN.</p>	 <p>PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.</p>
⑤		<p>PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS.</p>	<p>NOTES:</p> <ol style="list-style-type: none"> 1) MAY USE LAW ENFORCEMENT TO CONTROL TRAFFIC AT SIGNALIZED INTERSECTIONS AS DIRECTED BY THE ENGINEER. PROVIDE PORTABLE "ROAD WORK AHEAD" (W20-1) SIGNS 500' IN ADVANCE ALONG BOTH APPROACHES FROM THE SIDE STREETS WHEN PAVING PROCEEDS THROUGH THE INTERSECTION. 	

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**RESURFACING
ADVANCE WARNING SIGNS
FOR RURAL AND SUBURBAN
MULTI-LANE ROADWAYS
W/ SHOULDER SECTIONS**